

NOTES:

1. USE 4000 PSI CONCRETE @ 28 DAYS. CONCRETE SHALL BE A MIX DESIGN UTILIZING TYPE II PORTLAND CEMENT AND NORMAL WEIGHT AGGREGATES WITH 5-7% AIR ENTRAINMENT. NO FLY ASH WILL BE ALLOWED. MAXIMUM ALLOWABLE SLUMP SHALL BE 4 1/2". CONCRETE TO BE MECHANICALLY VIBRATED TO INSURE PROPER CONSOLIDATION.

2. USE A MIX DESIGN WHICH WILL DEVELOP 4000 PSI STRENGTH IN 7 DAYS ON THE APPROACHES.

3. ALL REINFORCING TO BE ASTM A-615 GR 60 DEFORMED BARS. ALL BAR TO BE LOCATED AS SHOWN ON PLANS WITH ALL INTERSECTING BAR SECURELY TIED AND CHAIRED INTO POSITION. BEND BARS COLD. REMOVE SCALE, LOOSE FLAKEY RUST, DIRT AND OTHER COATING THAT WOULD IMPAIR BONDING.

4. ALL CONCRETE SURFACES SHALL BE FORMED WITH WOOD OR METAL FORMS USING APPROVED METHODS. NO CONCRETE VERTICAL SURFACE SHALL BE POURED AGAINST DIRT.
5. MAKE 3 CONCRETE TEST CYLINDERS, LABEL WITH POUR DATE AND DESCRIPTION OF POUR, WRAP CYLINDERS IN 6 MIL POLY.

7. ALL WORK TO BE IN ACCORDANCE WITH ACI 301.


8. BOTTOM OF PIT SHOULD BE SUPPORTED ON NON-EXPANSIVE WELL DRAINED MATERIAL CAPABLE OF SAFELY SUPPORTING 1500 PSF WITH LESS THAN 3/4 INCH TOTAL SETTLEMENT.

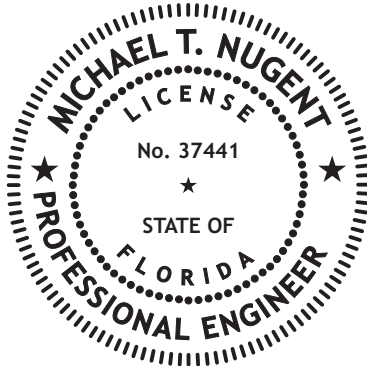
9. ALL BACKFILL SHOULD BE PLACED IN 8 INCH LIFTS AND COMPACTED TO 95% OF MAXIMUM DENSITY PER ASTM 1557 OR APPROVED ALTERNATE.

10. SUITABLE CONDUIT FOR LOW VOLTAGE CONDUCTOR SHIELDED CABLE MUST PASS THRU THE PIT WALL AS INDICATED ON THIS DRAWING. CONDUIT MUST EXTEND BEYOND PIT WALL 2" MIN.

11. WORK FROM CENTER LINES WHEN ERECTING FORMS AND PLACING FOUNDATION EMBEDMENTS. PLACE EMBEDMENTS TO WITHIN +/-1/8" OF POSITIONS INDICATED ON DRAWINGS. (EMBEDMENT MUST BE PLACED WITHIN +/-1/8" OF TOTAL DIMENSIONS OF SCALE.)
12. PROVIDE MEANS SO THAT ADEQUATE DRAINAGE FOR THE PIT EXISTS. UTILIZE SUMP PUMPS TO INSURE THAT NO STANDING WATER CAN ACCUMULATE IN THE PIT.

FAIRBANKS

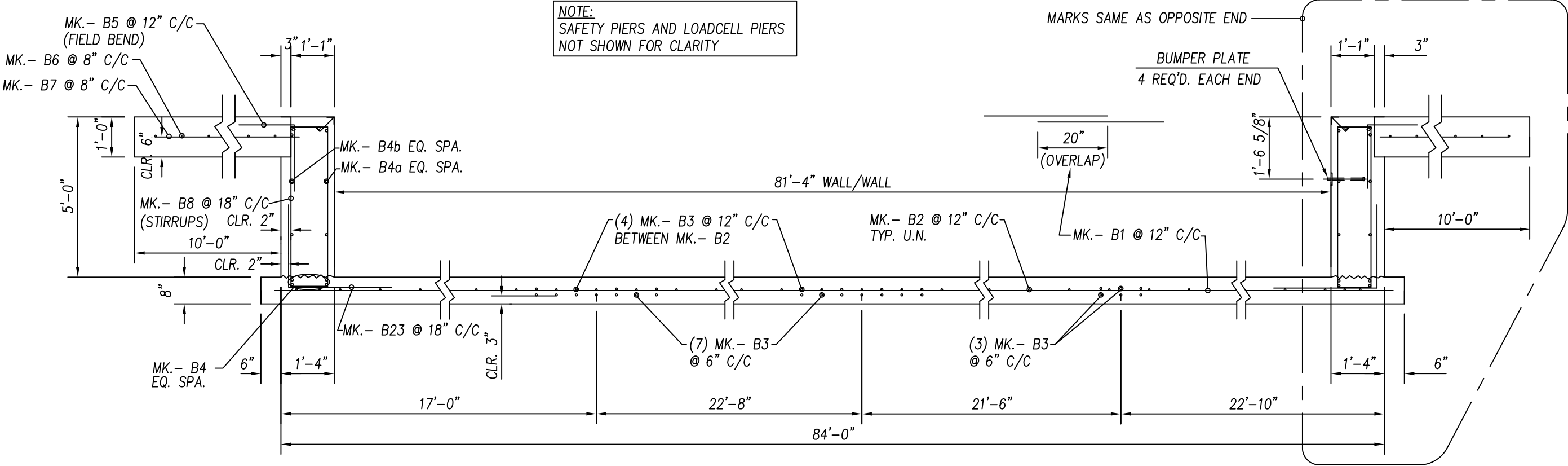
5					 <div>C.A.T. SCALE CO. P.O. Box 639 Walcott, Iowa 52773</div>					
4										
3										
2										
1										
REV.	DATE	REVISION DESCRIPTION		DR.	APPVD.	TITLE <div>Shuttle Truck Stop 75 Lake City, FL 32024 PLAN – 5’–0” DEEP SCALE PIT</div>				
REVISIONS						SCALE NONE	DR. CHK'D	TB MTN	DATE 1/2016	DRAWING NUMBER 1



DocuSigned by:
Michael Nugent
91CF4EC659214FA...

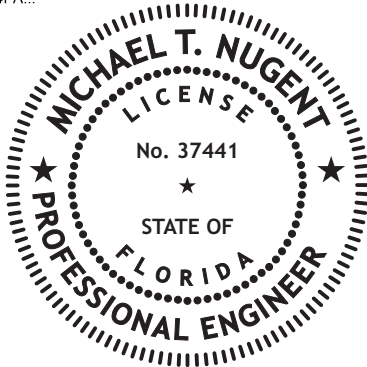
This item has been digitally signed and sealed by Michael T Nugent on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

8/19/20




SECTION A-A

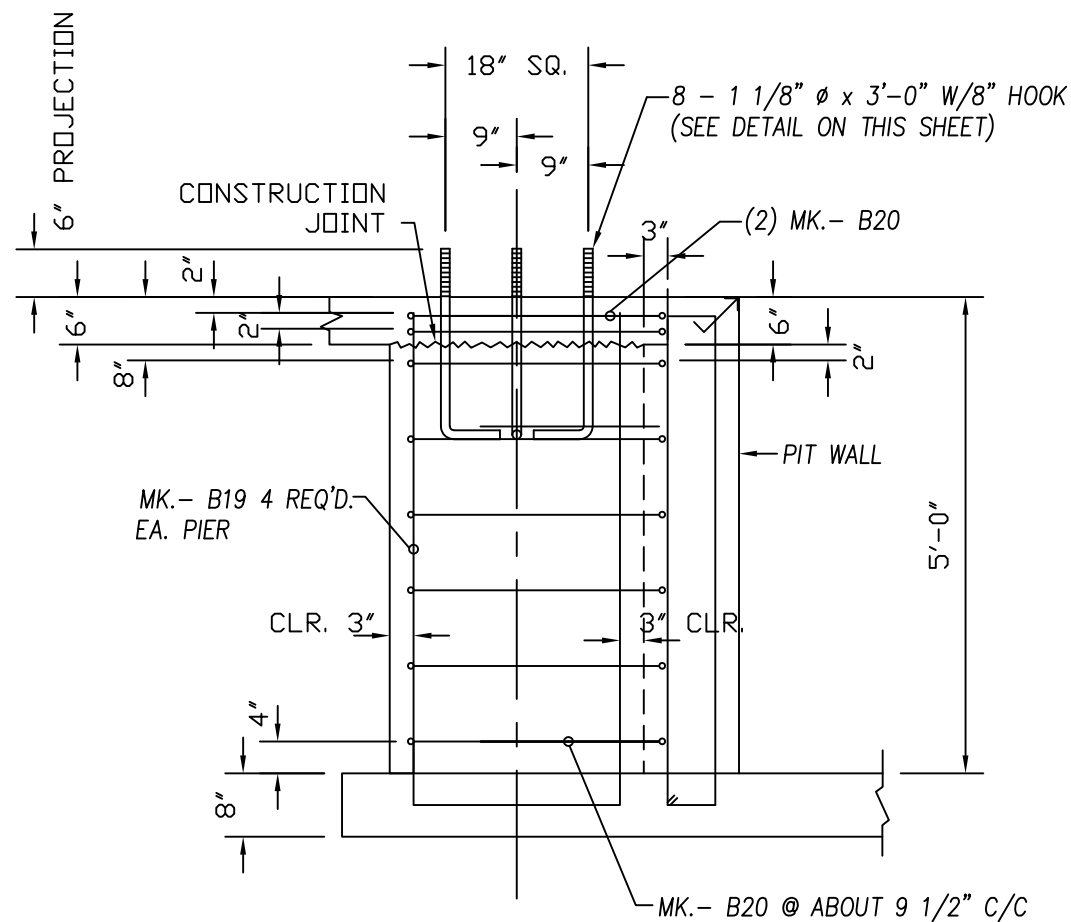
DocuSigned by:
Michael Nugent 8/19/20
91CF4EC659214FA...



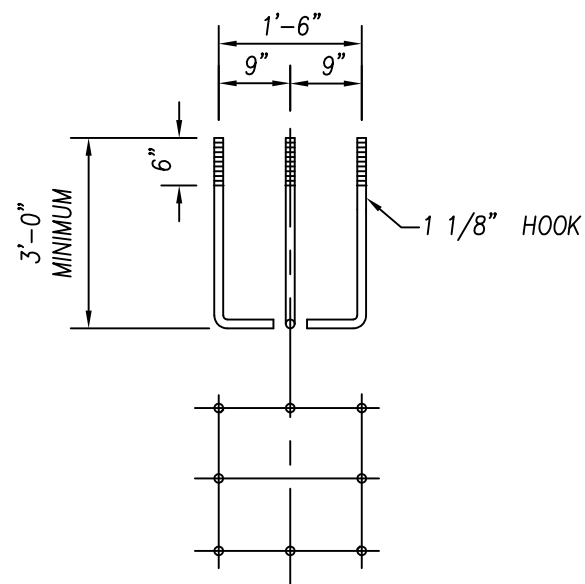
This item has been digitally signed and sealed by Michael T Nugent on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

FAIRBANKS

5					 <div>C.A.T. SCALE CO. P.O. Box 639 Walcott, Iowa 52773</div>				
4									
3									
2									
1									
REV.	DATE	REVISION DESCRIPTION		DR.	APPVD.	TITLE Shuttle Truck Stop 75 Lake City, FL 32024 <i>ELEVATION – 5’-0” DEEP SCALE PIT</i>			
REVISIONS						SCALE NONE	DR. TB CHK'D MTN	DATE 1/2016	DRAWING NUMBER 2

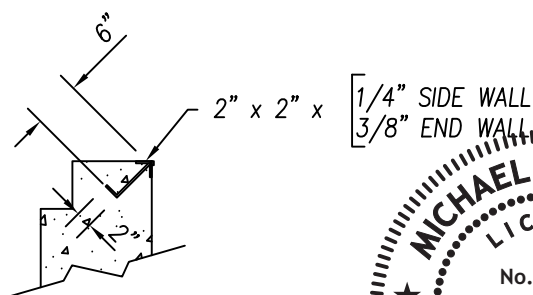


SECTION C
(SIGN FOUNDATION)



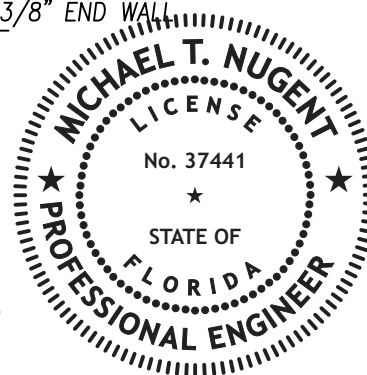
ANCHOR BOLT ASSEMBLY

This item has been digitally signed and sealed by Michael T Nugent on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

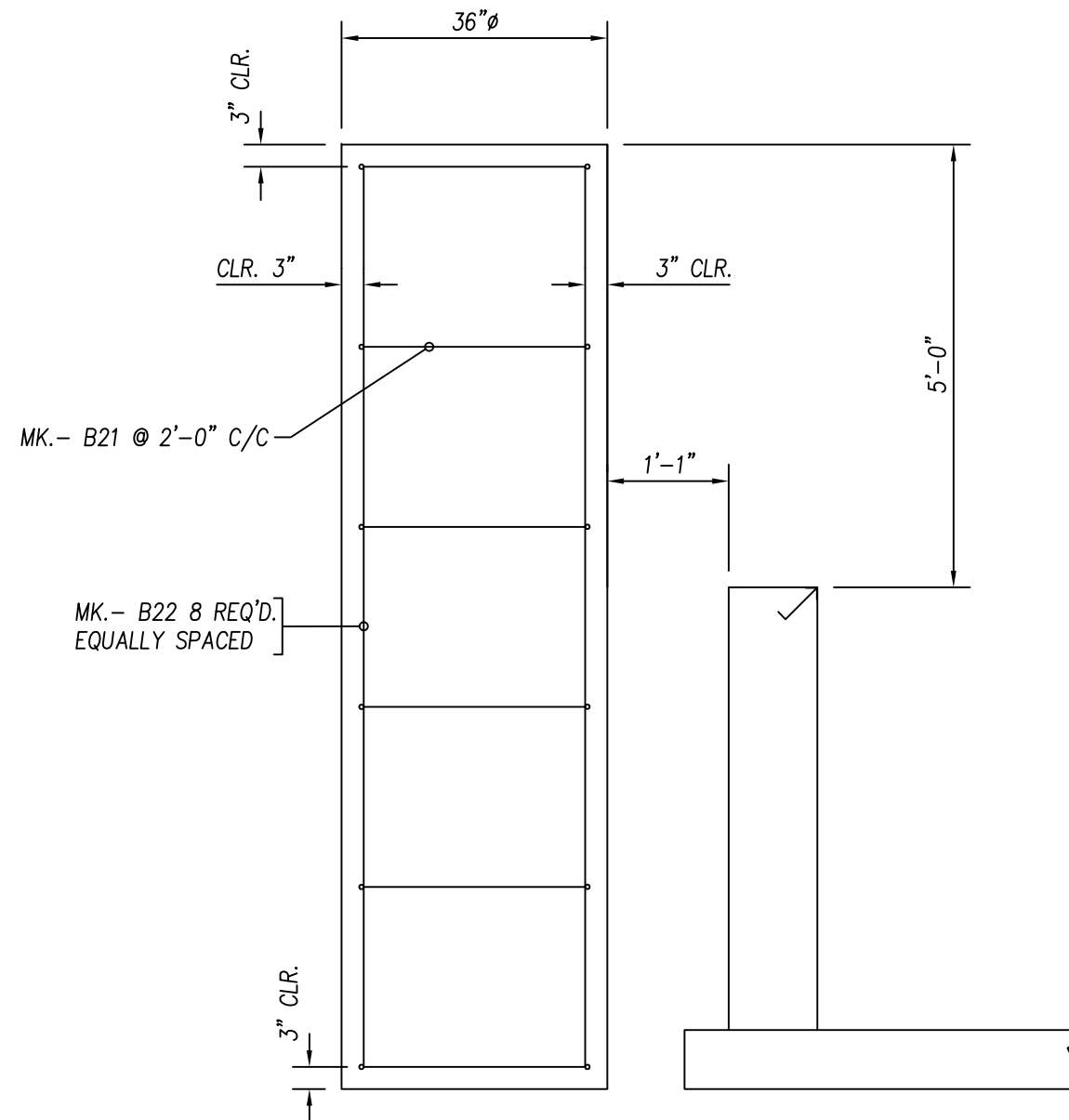


COPING FOUNDATION


8/19/20

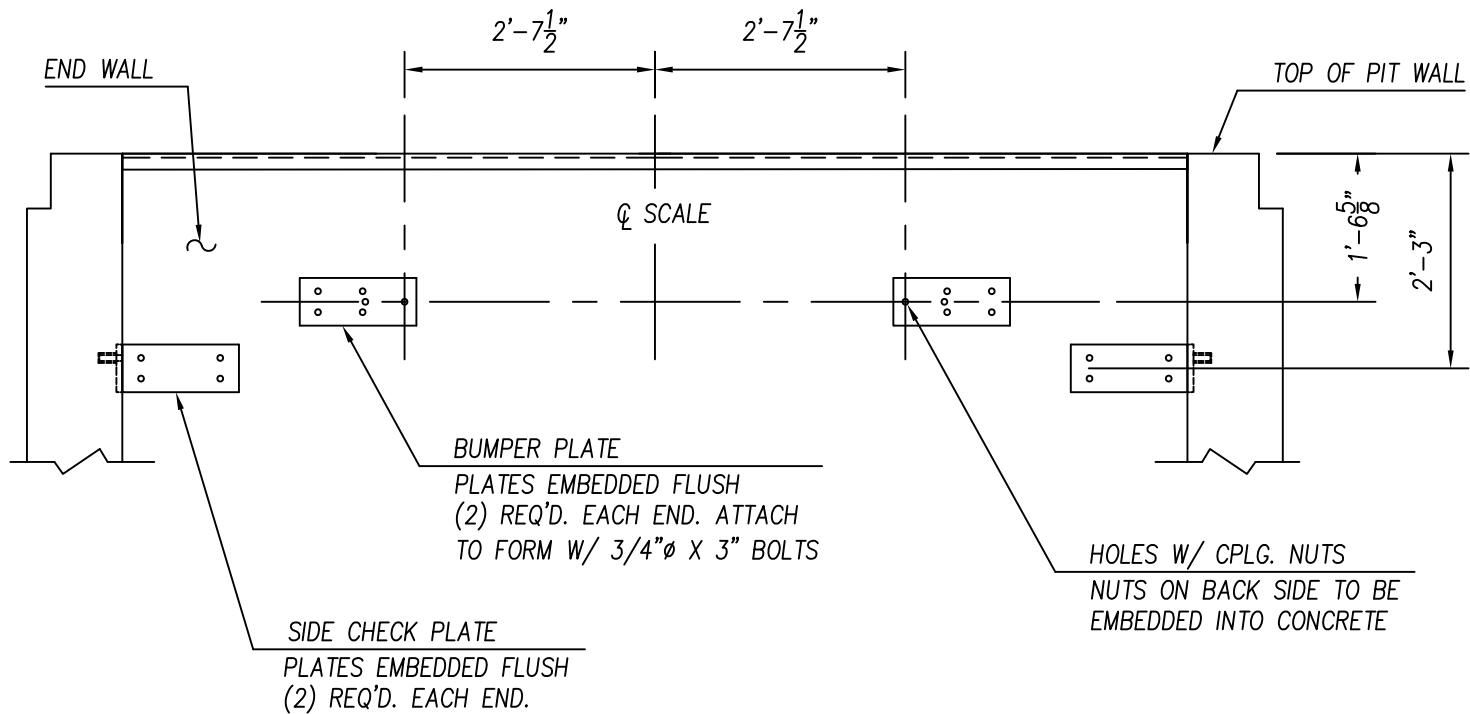


FAIRBANKS

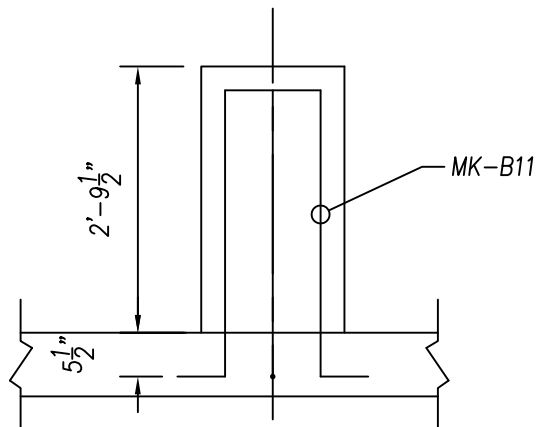


SECTION D
(TURNING BLOCK)

5					<div></div> <div>C.A.T. SCALE CO. P.O. Box 639 Walcott, Iowa 52773</div>										
4															
3															
2															
1															
REV.	DATE	REVISION DESCRIPTION	DR.	APPVD.	<div>TITLE</div> <div>Shuttle Truck Stop 75 Lake City, FL 32024</div> <div>SECTIONS C-C & D-D – CAT SCALE PIT</div>										
REVISIONS					<table><tr><td>SCALE</td><td>DR.</td><td>TB</td><td>DATE</td><td>DRAWING NUMBER</td></tr><tr><td>NONE</td><td>CHK'D</td><td>MTN</td><td>1/2016</td><td>4</td></tr></table>	SCALE	DR.	TB	DATE	DRAWING NUMBER	NONE	CHK'D	MTN	1/2016	4
SCALE	DR.	TB	DATE	DRAWING NUMBER											
NONE	CHK'D	MTN	1/2016	4											



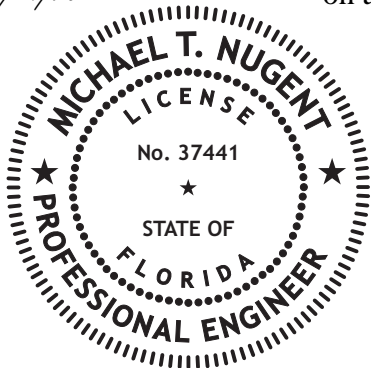
DETAIL – END WALL CHECKING



DETAIL – SAFETY PIER

This item has been digitally signed and sealed by Michael T Nugent on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.


8/19/20



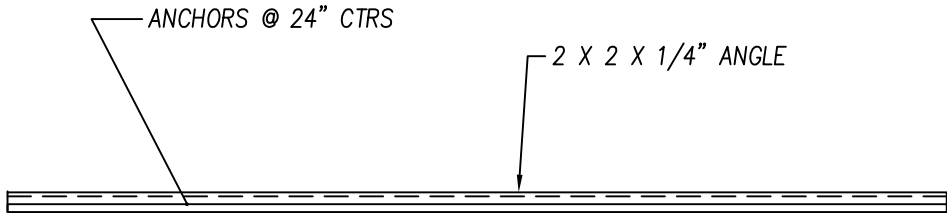
DocuSigned by:

Michael Nugent

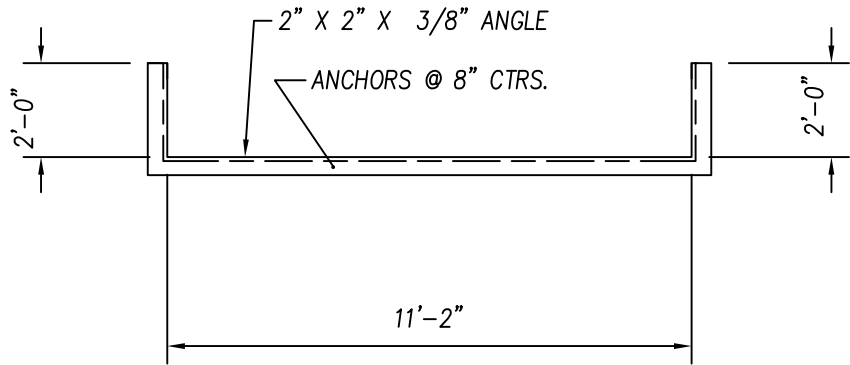
91CF4EC659214FA...

5					 C.A.T. SCALE CO. P.O. Box 639 Walcott, Iowa 52773
4					
3					
2					
1					
REV.	DATE	REVISION DESCRIPTION	DR.	APPVD.	TITLE
REVISIONS					Shuttle Truck Stop 75 Lake City, FL 32024
					DETAILS – WALL CHECKING & SAFETY PIERS
SCALE		DR.	TB	DATE	DRAWING NUMBER
NONE		CHK'D	MTN	1/2016	5

FAIRBANKS



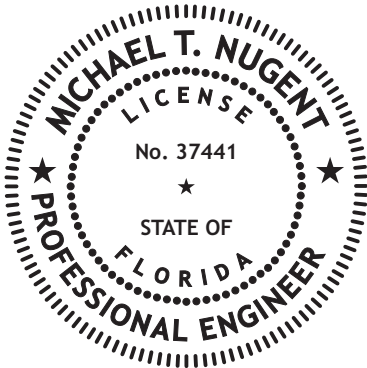
TYPICAL 20'-0" COPING
8 - REQ'D.



TYPICAL END WALL COPING
2 - REQ'D.

This item has been digitally signed and sealed by Michael T Nugent on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

DocuSigned by:
Michael Nugent
91CF4EC659214FA...



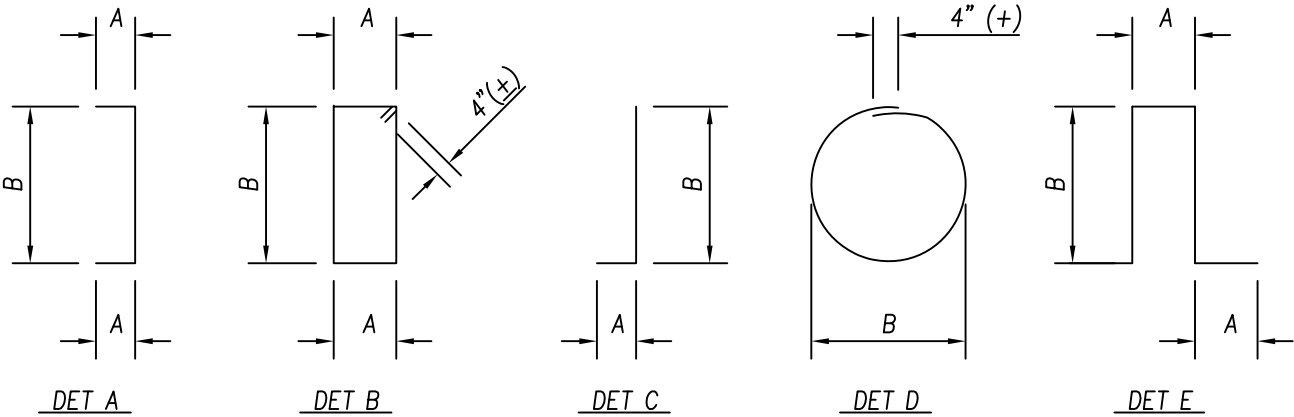
8/19/20

FAIRBANKS

REINFORCING STEEL SCHEDULE - 5'-0" PIT

MARK	QTY	DESCRIPTION	LENGTH	A	B
B1	60	# 4 STRAIGHT	22'-6"		
B2	86	# 4 STRAIGHT	13'-10"		
B3	28	# 5 STRAIGHT	13'-10"		
B4a	12	# 6 BENT - DET A	15'-9"	2'-0"	11'-9"
B4b	12	# 6 BENT - DET A	16'-8"	2'-0"	12'-8"
B5	188	# 4 STRAIGHT	6'-6"		
B6	30	# 4 STRAIGHT	12'-2"		
B7	40	# 4 STRAIGHT	9'-10"		
B8	18	# 4 STIRRUPS - DET B	12'-8"	0'-11"	5'-1"
B9	96	# 5 STRAIGHT	22'-6"		
B10	110	# 4 STIRRUPS - DET B	12'-0"	0'-7"	5'-1"
B11	32	# 4 BENT - DET E	8'-6"	0'-10"	3'-0"
B12	50	# 4 STRAIGHT	22'-6"		
*B13	40	# 4 STRAIGHT	4'-0"		
B14	8	# 5 TIES - DET B	14'-8"	4'-6"	2'-6"
B15	4	# 5 TIES - DET B	8'-8"	1'-6"	2'-6"
B16	8	# 5 TIES - DET B	9'-8"	2'-0"	2'-6"
B17	40	# 4 BENT - DET C	2'-10"	1'-0"	1'-10"
B18	72	# 6 BENT - DET C	3'-3"	1'-0"	2'-3"
B19	8	# 6 BENT - DET C	6'-2"	1'-0"	5'-2"
B20	16	# 5 TIES - DET B	10'-4"	2'-8"	2'-2"
B21	6	# 5 HOOP - DET D	8'-3"	(DIA.)	2'-6"
B22	8	# 6 STRAIGHT	10'-2"		
B23	128	# 4 BENT - DET C	5'-0"	2'-0"	3'-0"

*MK B13 IS SUPPLEMENTARY AND IS NOT PART OF THE SCALE PIT REBAR



NOTE:
ALL REINFORCING STEEL TO BE ASTM A615 GRADE 60 DEFORMED.
ALL BENDING MUST BE COLD FORMED.

5					<div>CAT SCALE</div> <div>C.A.T. SCALE CO. P.O. Box 639 Walcott, Iowa 52773</div> <div>TITLE</div> <div>Shuttle Truck Stop 75 Lake City, FL 32024</div> <div>COPING DETAIL & REINFORCING STEEL DETAIL</div>			
4								
3								
2								
1								
REV.	DATE	REVISION DESCRIPTION	DR.	APPVD.	SCALE	DR. TB	DATE	DRAWING NUMBER
REVISIONS					NONE	CHK'D MTN	1/2016	6